

6.8 Transition

6.8.1 Illustrations and Intent

Note: These are provided as illustrations of intent. The illustrations and statements on this page are advisory only and do not have the power of law. Refer to the standards on the following pages for the specific Building Form and Development Standards.

The Transition Zone building form and development standards are intended to address development between all other character zones and existing residential areas. Development standards in this district are intended to appropriately blend the areas between new construction and existing buildings outside of the character zones.

Generally, this district may accommodate low-rise commercial and urban residential development. The goal is to have a reasonable buffer between existing buildings and any higher density new construction.

In addition, the site shall be planned in such a manner as to facilitate residential scale buildings converted to office and small scale retail uses and live-work, discouraging paving of side and backyards for parking, allow for low intensity office and neighborhood services and retail uses, and retain a 1 to 2 story, small neighborhood scale.



The transition commercial to residential is buffered by urban residential apartments and townhomes.

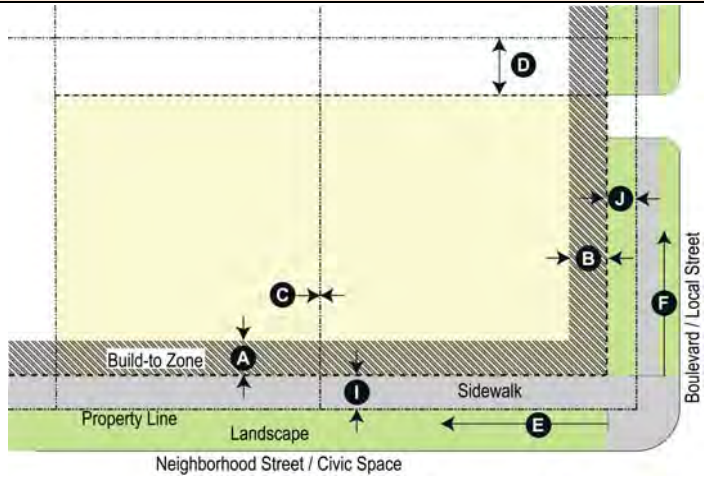


An office development with appropriate frontages along Camp Bowie and a surface parking area, screened from the public realm.



Transition Zone – Recommended Block and Building Layout

6.8.2 Building Placement



(i) Build-To-Zone (BTZ)

Front - Neighborhood Street / Civic Space	5' – 10' (see #1)	A
Front - Boulevard and Local Streets	10' – 30' (see #2)	B

(ii) Setback

Front (Neighborhood Street and Civic Space)	5' (min.) 10' (max.)	I
Front (Boulevard and Local Streets)	10' (min.) 30' (max.)	J
Side or Rear (distance from property line)	0' (see #3 and 6.8.7(iii))	C D

(iii) Building Frontage Required

% of building built to Neighborhood Street BTZ	75% (min.) (see #4 and #7)	E
% of building built to Boulevard and Local Streets BTZ	30% (min.) (see #4 and #7)	F

6.8.3 Block Standards

Block Face Dimensions	250' (min.) 600' (max.)
Block Perimeter	2000' (max.)

Principal Building Standards

Building Maximum	3 stories (max.) (see #6 and #8)	K
First Floor to Floor Height	15' (min.)	M
Ground Floor finish level	12 inches max. above sidewalk (for ground floor Commercial Ready buildings)	L
Upper floor to floor height	10' (min.) (see #5)	N

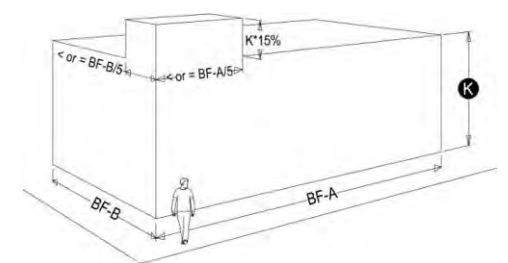
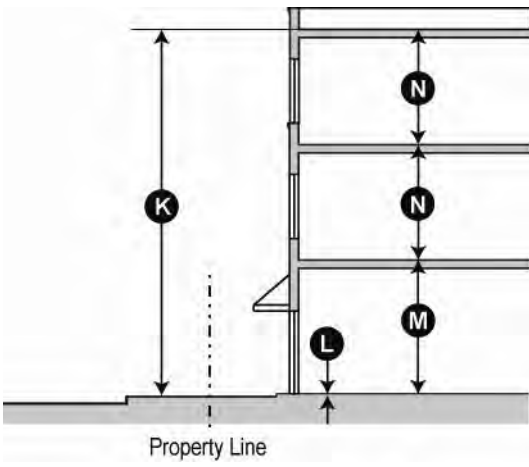
6.8.5 Commercial Frontage Requirements

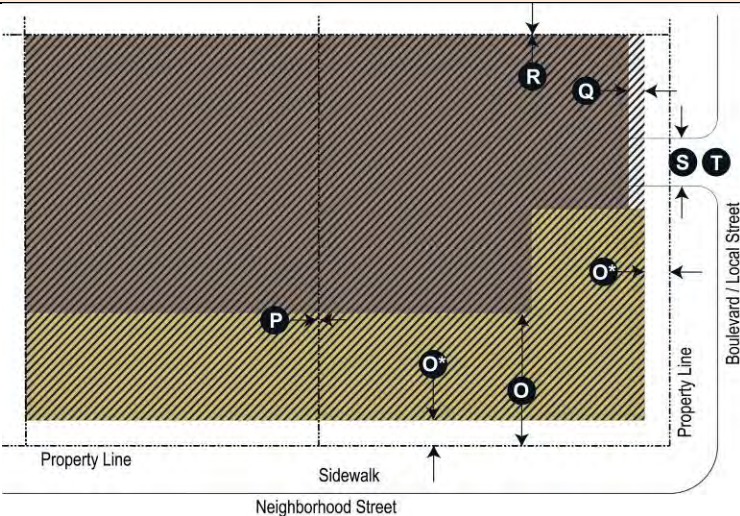
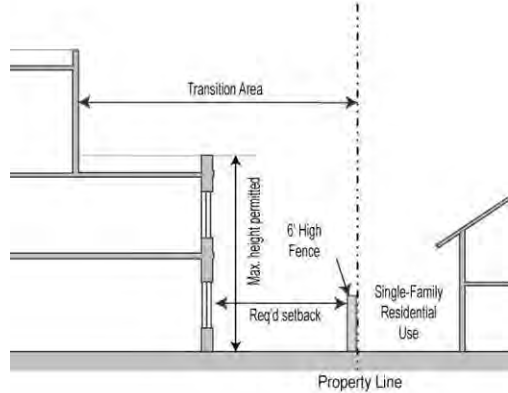
Ground floors of all buildings fronting on Camp Bowie Blvd. shall be built to Commercial Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

Notes

- #1 – Area between the building and the edge of the BTZ at the sidewalk shall be paved flush with the public sidewalk.
- #2 – The area between the building and the edge of the BTZ at the public sidewalk shall include a 6' wide (min.) landscaping strip with street trees planted at 40' on center (average), except at street intersections, where paving is optional up to 30' along the building façade. Species of the street trees shall be selected from the Planting List in the Zoning Ordinance.
- #3 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #4 – Corner building street facades shall be built to the BTZ for a minimum of 30' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #5 – Floor to floor heights shall not apply to parking structures.
- #6 – Attics and mezzanines less than 7 feet (avg.) height shall not be counted as a story.
- #7 – Any frontage along all Neighborhood Streets and the Boulevard not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen. Furthermore, along all streets (Neighborhood Streets, Boulevard and Local Streets) service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. Required Street Screens shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Planting List in Table 6.8 Table A in Chapter 6 Development Standards of the City of Fort Worth Zoning Ordinance. The required Street Screen shall be located at the setback line along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.

6.8.4 Height Standards



<p>6.8.6 Parking and Service Access</p>  <p>Legend</p> <ul style="list-style-type: none"> --- Property Line ■ Building Footprint ▨ Surface Parking Area ▩ Above Grade Parking Area 	<p><i>(i) Parking Location</i></p> <table border="1"> <tr> <th colspan="3">Surface/At Grade Parking</th> </tr> <tr> <td>Neighborhood Street and Civic Space setback</td> <td>Shall be located behind the principal building</td> <td>O</td> </tr> <tr> <td>Boulevard and Local Street setback</td> <td>Min. of 3 feet behind the building facade line along that street</td> <td>Q</td> </tr> <tr> <td>Side and Rear setback (distance from property line)</td> <td>0' min. (see # 3 and 6.8.7(iii))</td> <td>P R</td> </tr> <tr> <th colspan="3">Above Grade Parking</th> </tr> <tr> <td>Setback along Neighborhood Street, Type Boulevard, Local Street or Civic Space</td> <td>May be built up to the building façade line along that street</td> <td>O*</td> </tr> <tr> <td>Side and Rear setbacks (distance from property line)</td> <td>0' min. (see # 3 and 6.8.7(iii))</td> <td>P</td> </tr> <tr> <th colspan="3"><i>(ii) Required Off-Street Parking Spaces **</i></th> </tr> <tr> <td>Non-residential uses within 250' of single-family residential</td> <td>1 space per every 300 square feet (gross)</td> <td></td> </tr> <tr> <td>Residential Uses</td> <td>1.5 space/unit</td> <td></td> </tr> <tr> <td colspan="3">** Uses in historically significant buildings are exempt</td> </tr> </table>	Surface/At Grade Parking			Neighborhood Street and Civic Space setback	Shall be located behind the principal building	O	Boulevard and Local Street setback	Min. of 3 feet behind the building facade line along that street	Q	Side and Rear setback (distance from property line)	0' min. (see # 3 and 6.8.7(iii))	P R	Above Grade Parking			Setback along Neighborhood Street, Type Boulevard, Local Street or Civic Space	May be built up to the building façade line along that street	O*	Side and Rear setbacks (distance from property line)	0' min. (see # 3 and 6.8.7(iii))	P	<i>(ii) Required Off-Street Parking Spaces **</i>			Non-residential uses within 250' of single-family residential	1 space per every 300 square feet (gross)		Residential Uses	1.5 space/unit		** Uses in historically significant buildings are exempt			<p>6.8.7 Other Standards</p> <p>(i) Encroachments</p> <ol style="list-style-type: none"> Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane. Building projections on all other facades may not be closer than 5' to any adjacent property line. Encroachments must be approved administratively or by City Council. <p>(ii) Arcades and Colonnades:</p> <ol style="list-style-type: none"> Arcades and Colonnades are permitted within the build-to-zone and shall be a minimum of 6' in depth. The minimum interior clearance height within an arcade or colonnade shall be 14'. <p>(iii) Neighborhood Transitions:</p> <ol style="list-style-type: none"> A Neighborhood Transition Zone shall be established on all Highway Frontage Sites at 25' parallel to any lot line that is common with a single-family residential lot. Building height within this Neighborhood Transition Zone shall not exceed 2 floors. This standard shall apply to any parking structures located within the Neighborhood Transition Zone. A privacy fence (masonry or opaque vegetative) of a 6' height shall be required when abutting a single-family residential lot and shall be optional for all other adjacencies.
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<p>6.8.8 Façade Elements</p> <p>(i) Doors and Windows:</p> <ol style="list-style-type: none"> There shall be no blank walls greater than 30 ft. in width of any building along Neighborhood Streets and the Boulevard only. For Local Streets there is no requirement. Doors and windows on ground floor of all buildings shall be a minimum of 25% of the ground floor façade area along all Neighborhood Streets and the Boulevard. Along Local Streets there is no requirement. Windows on the upper floors facades along all Neighborhood Streets and the Boulevard shall be a minimum of 20% of each upper floor façade area which is measured between 3ft. and 9ft. above each finished floor. Primary entrance doors for all buildings shall be on Neighborhood Streets or along the Boulevard. 	<p><i>(ii) Driveways and Service Access</i></p> <table border="1"> <tr> <td>Parking driveway width</td> <td>TxDOT standards on Arterial Roadways and 24 feet max. on all other streets</td> <td>S</td> </tr> <tr> <td colspan="3">Driveways and off-street loading and unloading shall not be located on Neighborhood Streets.</td> </tr> <tr> <td colspan="3">Porte cocheres may be permitted on Neighborhood Streets to provide drop-off and valet service.</td> </tr> <tr> <td colspan="3">Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.</td> </tr> <tr> <td colspan="3">If driveway and/or off-street service loading and unloading access is provided from Neighborhood Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.</td> </tr> </table>	Parking driveway width	TxDOT standards on Arterial Roadways and 24 feet max. on all other streets	S	Driveways and off-street loading and unloading shall not be located on Neighborhood Streets.			Porte cocheres may be permitted on Neighborhood Streets to provide drop-off and valet service.			Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.			If driveway and/or off-street service loading and unloading access is provided from Neighborhood Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.																					
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