

Parking and Driveways

Intent:

Landscaping is an important tool to mitigate the visual impact of parking. The intent of this section is to ensure that landscaping is appropriately used to mitigate surface parking and to provide flexibility, if necessary, for developers to meet unique site conditions.

Guidelines:

- Pedestrian walkways are encouraged to offer safe, marked routes between parking spaces and building entries. (See Figure 19.1)
- When possible, utilize shared parking provisions to reduce the total number of parking spaces.
- Parking islands are recommended to achieve the required canopy coverage and to breakup long rows of parking.

Standards:

- All parking areas in all zones shall be limited to a maximum of 200 spaces per parking “room”.
- For every parking area a minimum ten (10) foot landscaped median/divider shall be installed to help break up “rooms” of large parking fields. (See Figure 18.1)
- For buildings fronting I-35W, a minimum of 50% of the parking areas shall be located behind or within groups of buildings. (See Figure 18.2)
 - If 50% of parking cannot be located behind the building or within groups of buildings, one tree shall be added for every additional ten (10) spaces, beginning with the first additional space and for each ten (10) spaces thereafter. Priority should be given to placing additional trees within the paving or building setback fronting I-35W.
 - If buildings front I-35W and are located outside the design overlay, all surface parking located within the overlay fronting I-35W will be required to meet the planting conditions above.
- The setback areas along I-35W and where parking areas and/or drives front the public right-of-way will be required to include earthen berms for screening purposes where topography allows. If topography does not allow, refer to page 22 for alternate screening methods. (Reference pages 20 and 22)
- All parking areas shall have buffers of at least ten (10) feet from any building face fronting on a street and ten (10) feet from side and rear building faces to allow for a combination of landscaping, sidewalks, and pedestrian plazas. No less than 30% of the remaining area in the 10 foot buffer must be landscaping from the approved planting list (Page 29) and distributed along the front and sides of the building. (See Figure 21.2) Where appropriate in urban and retail settings a combination of planters and planting beds can be used to achieve the required landscaping. Paving requirements vary by zone, refer to pages 26-28.

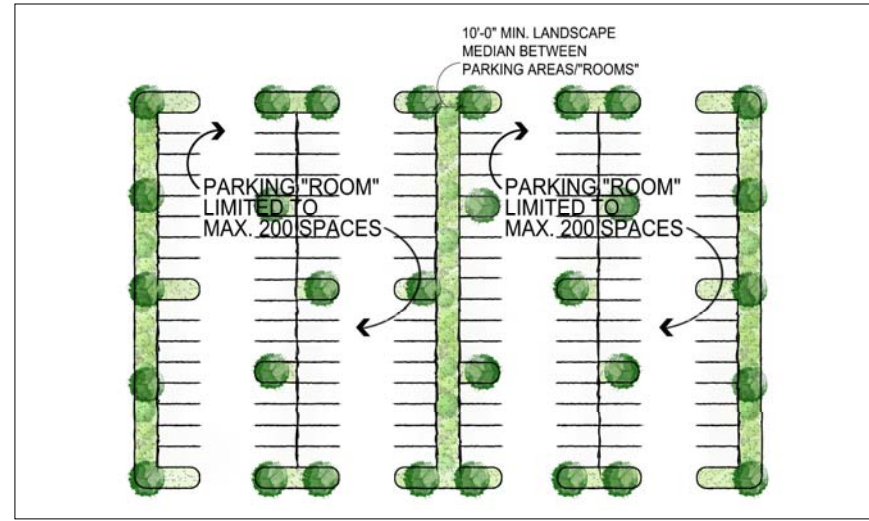


Fig. 18.1 Diagram example of parking rooms

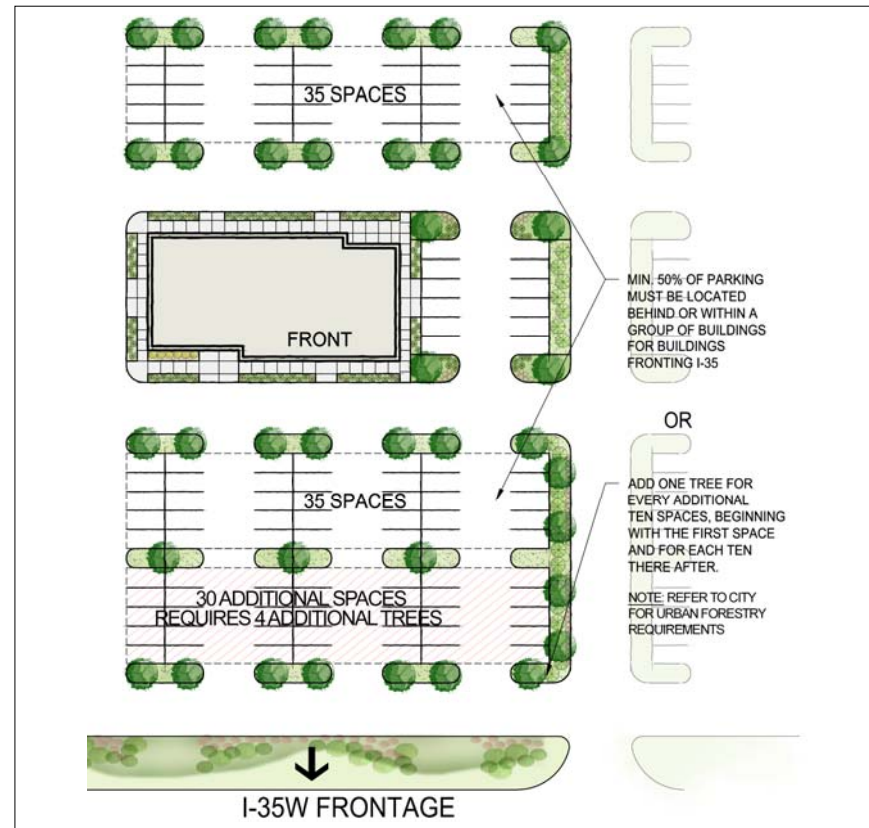


Fig. 18.2 Diagram example of parking requirements when fronting I-35W

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Standards (continued):

- Landscape screening is required between the parking areas on adjacent parcels so as to break up the area of paving; landscape screening shall be a minimum of ten (10) feet wide and accomplished with earthen berms and/or with planting material from the approved planting list (See page 22 and 29). If there is a shared access easement along the property line, a minimum ten (10) foot landscaped median/divider shall be installed on either side of the access easement. (See Figures 19.2-19.3)
- Earthen berms shall be a natural, oscillating form with an average overall height of thirty-six (36) inches tall. (Refer to page 22)
- Sidewalks, medians, and building entryways shall utilize appropriate paving and landscaping materials. All pavers in public spaces and common areas shall be interlocking concrete pavers—terra cotta, pewter, natural/limestone or equal.



Terra Cotta



Natural/Limestone



Pewter

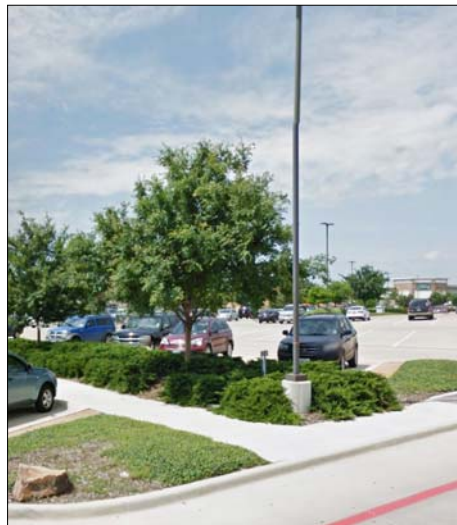


Fig. 19.1 Examples of pedestrian walkways along landscape medians



Fig. 19.2 Example of shared access easement along the property line

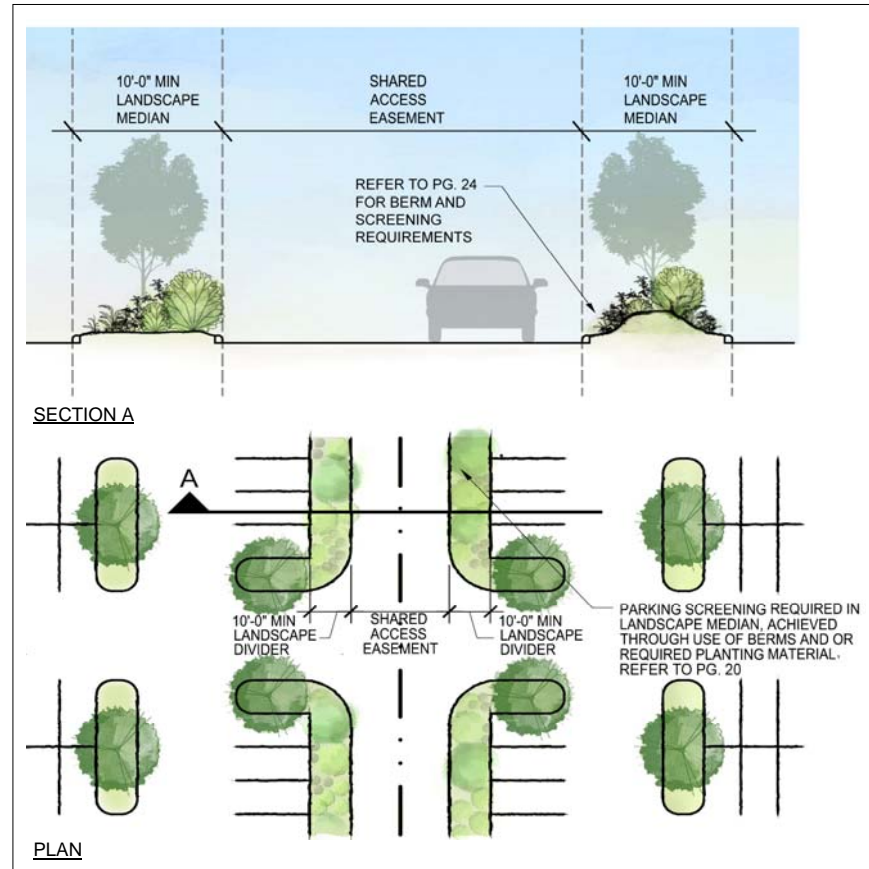


Fig. 19.3 Diagram example of a shared access easement along the property line

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Fig. 20.1 Example of landscape screening within the paving setback



Fig. 20.4 Example of compatible earthen berm with landscaping

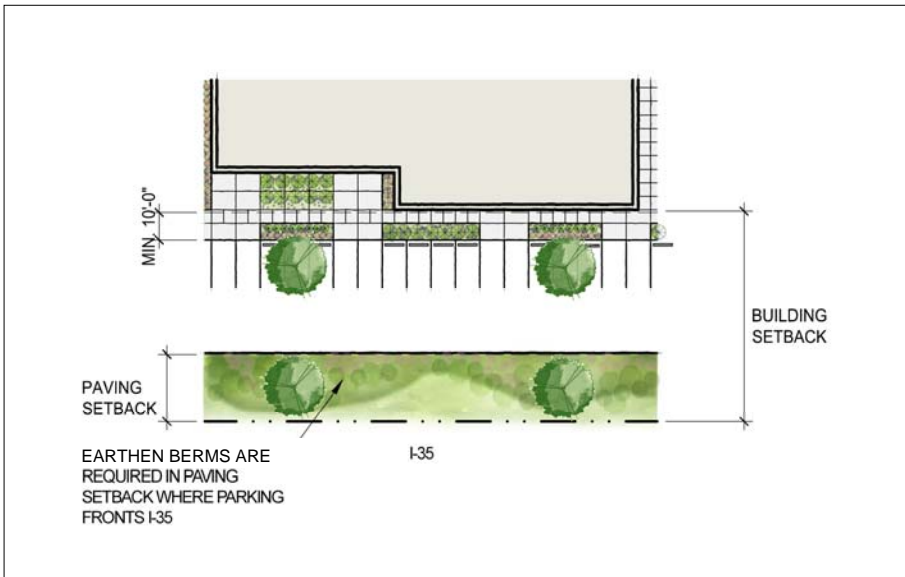


Fig. 20.2 Diagram of how setbacks are measured

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Fig. 21.1 Example of distributing landscaping along parking buffers



Fig. 21.3 Example of a successful combination of landscaping, sidewalk and paving

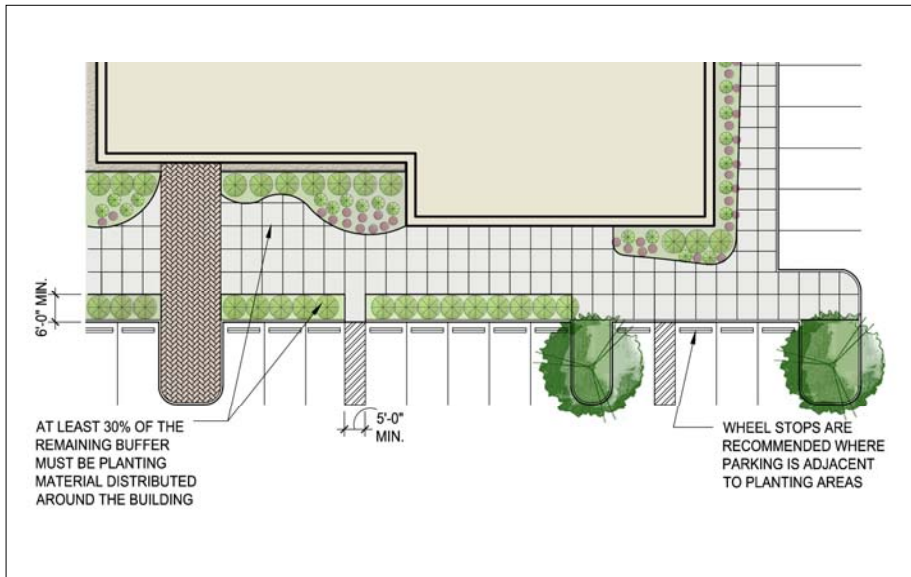


Fig. 21.2 Diagram of parking buffers where building fronts the street