

## WATERWAYS

### Intent

This section illustrates the Panther Island water system – a flood protection and water quality enhancement system that includes a bypass channel, urban lake, urban riverfront, and interior canals. The illustrations also include cross-sections and development guidelines for the various water bodies. Water serves as the unifying development concept for Panther Island, and all development should contribute to the integrity of the water system and be consistent with the principles listed below.

### Waterfront Access Principles

- Ensure a continuity of public access to and along waterfronts.
- Provide pedestrian and view corridors that allow physical and visual access to the waterfront.
- Clearly define access points and edges of waterfront spaces to facilitate public use and security, and to clearly distinguish between public and semi-public or private spaces.
- Accommodate multiple travel modes along the waterfront, including pedestrians, bicycles (slower speeds along urban waterfronts), and water taxis.

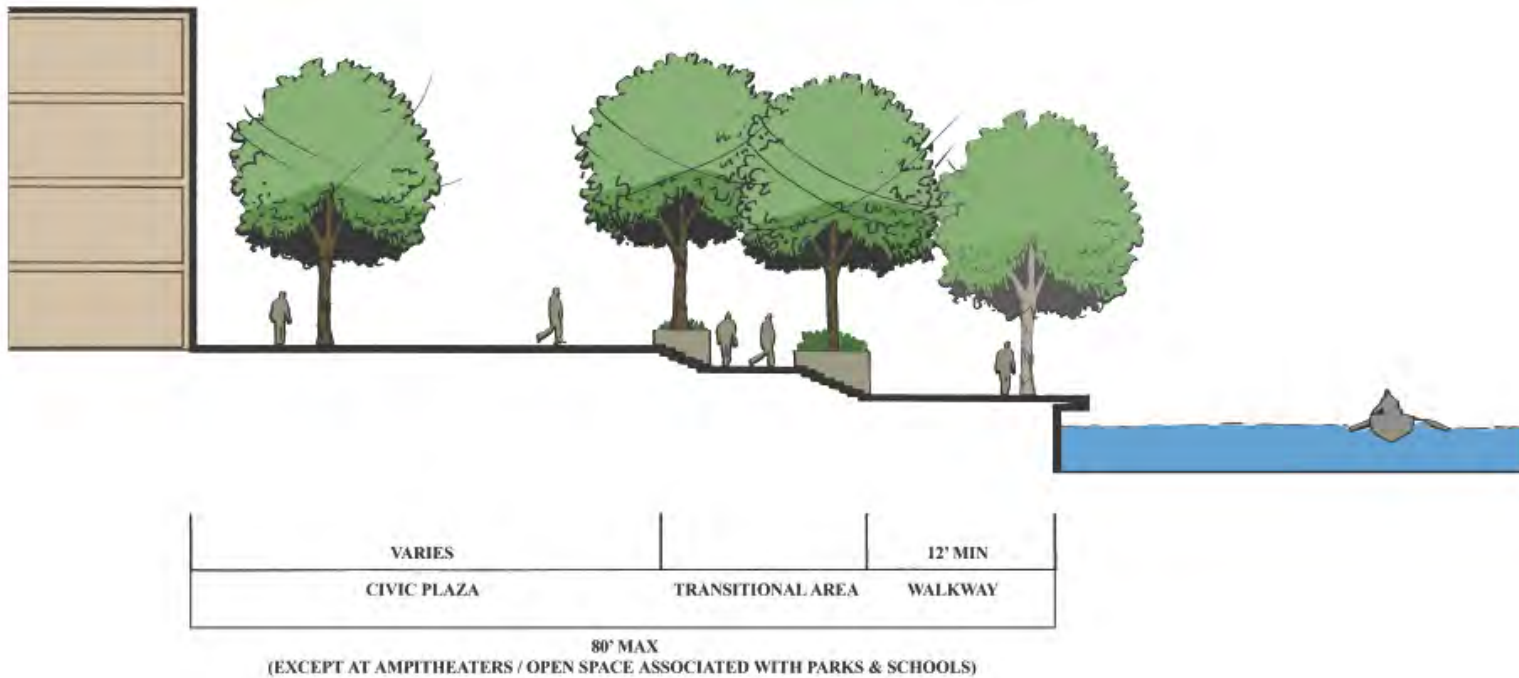
### Waterfront Experience Principles

- Build a distinctive and diverse network of public spaces and waterfront environments that avoid long uninterrupted monotonous designs.
- Include both informal and formal spaces and their neighborhood connectors to reflect or complement the adjoining neighborhoods character.
- Ensure comfort during hot weather with trees and other provisions for shade, water fountains, benches, and areas for rest.
- Ensure that buildings and other components of the built environment frame and complement waterfronts and public spaces.

San Antonio River walk



## URBAN LAKE AND RIVER CHANNEL - PUBLIC PLAZA SECTION



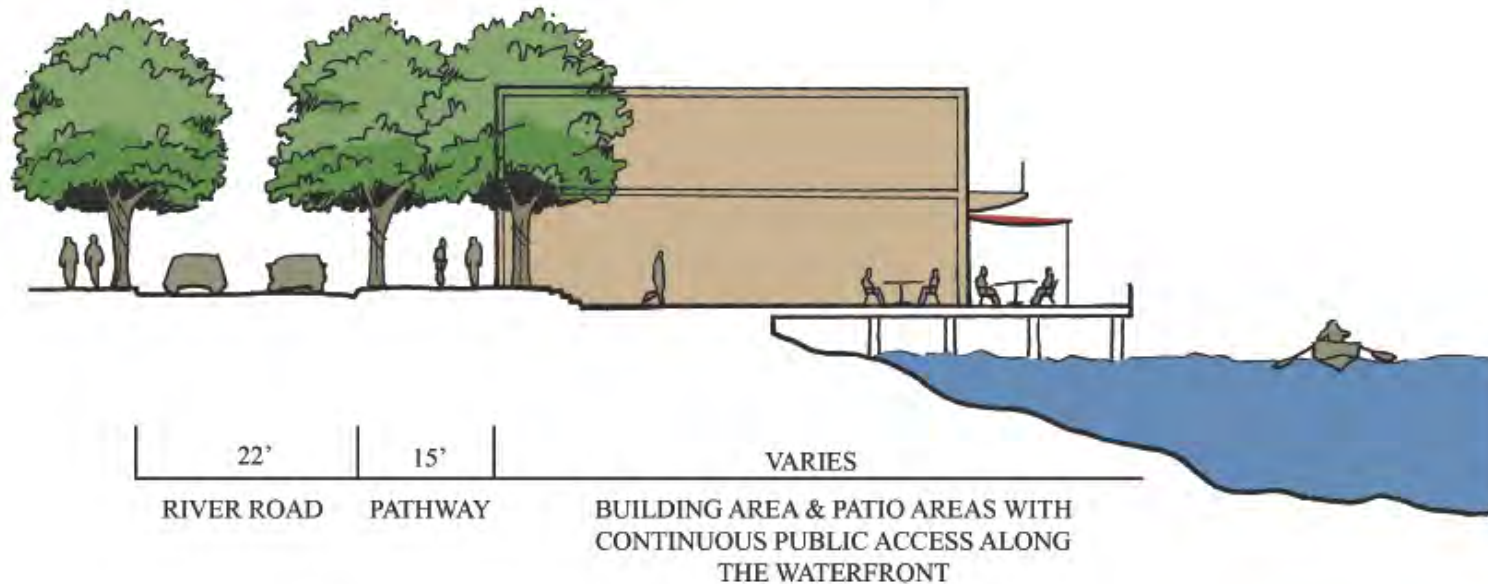
### Standards:

- Provide walkway width of 12-18 feet; maximum may be exceeded for civic spaces, parks, and schools
- Provide continuous public access along the Urban Lake.

### Guidelines:

- Encourage public plazas/spaces that accommodate community events in appropriate locations, such as the urban lake area.
- Encourage active uses and outdoor vendors.
- Provide intimate and comfortable places for relaxing, fishing, and canoe/kayak access.
- Provide shaded areas to promote activity during hot summers.
- Encourage outdoor dining.
- Integrate stops for water taxi service.

## URBAN LAKE AND RIVER CHANNEL - STANDARD SECTION FOR THE RIVER ROAD



### Standards:

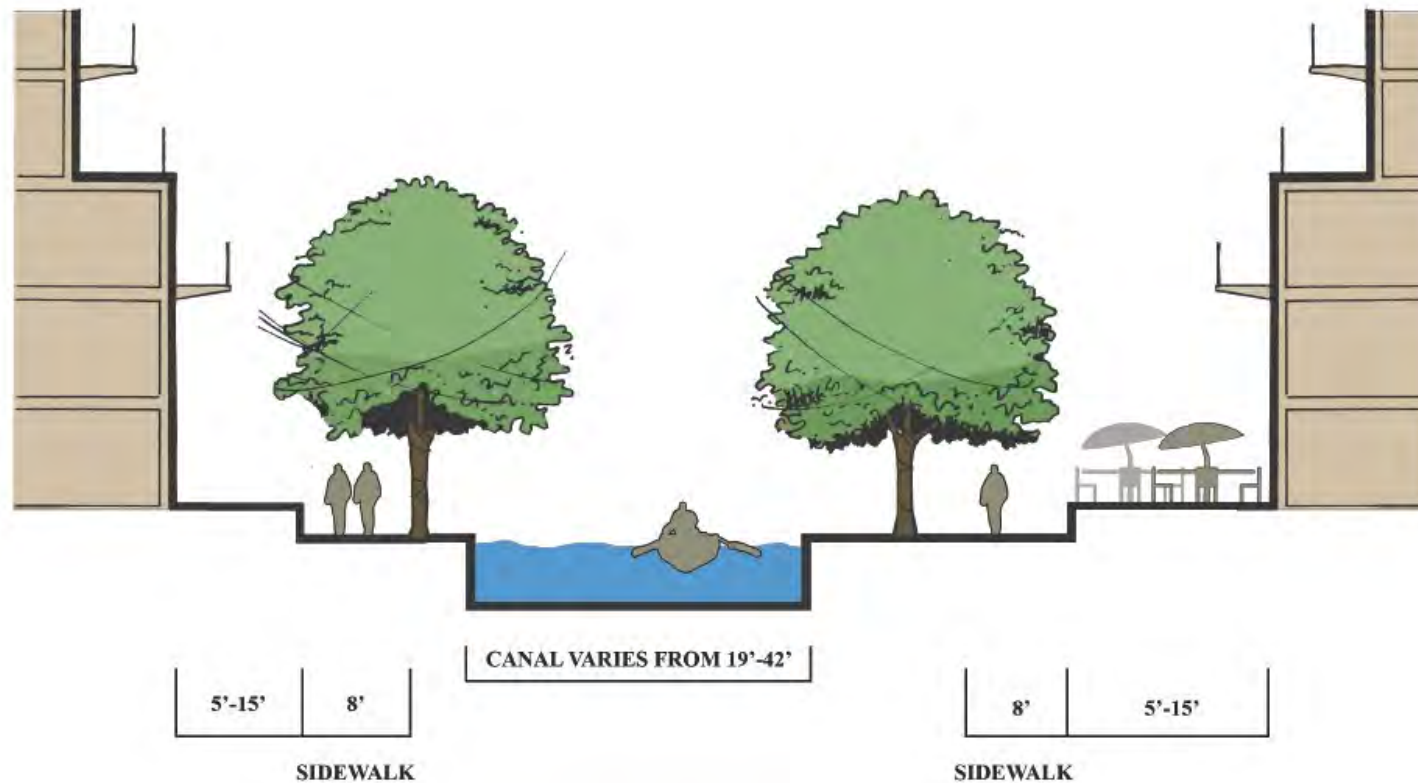
- Provide walkway width of 12-18 feet; maximum may be exceeded for civic spaces, parks, and schools.
- Provide continuous public access along the Urban Lake.
- Street Trees 25 feet

### Guidelines:

- Encourage public plazas/spaces that accommodate community events in appropriate locations, such as the urban lake area.
- Encourage active uses and outdoor vendors.
- Provide intimate and comfortable places for relaxing, fishing, and canoe/kayak access.
- Provide shaded areas to promote activity during hot summers.
- Encourage outdoor dining.
- Integrate stops for water taxi service.
- In certain areas, a meandering pathway may be appropriate.



## CANALS



### Standards:

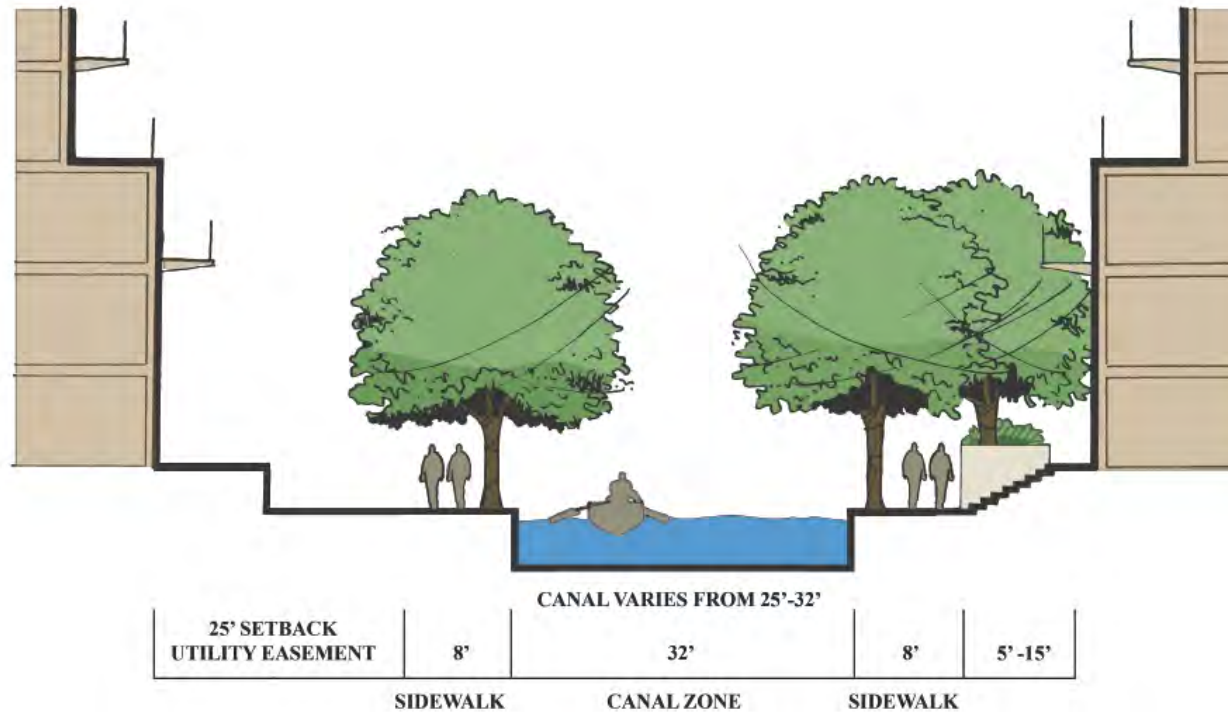
- Provide an 8-foot wide pedestrian area.
- Provide at least 3' of freeboard between the edge of the canal and the finished floor of development, outdoor dining or open space.
- Canals shall vary in width (from 19'- 42') throughout the canal zone depicted above.
- Provide continuous public access along canals, at least on one side.

### Guidelines:

- Encourage balconies and other features that provide visual interest and a strong relationship between buildings and canal walkways.



## CANAL-CLEAR FORK SOUTH WITH UTILITY EASEMENT TO THE NORTH



### Standards:

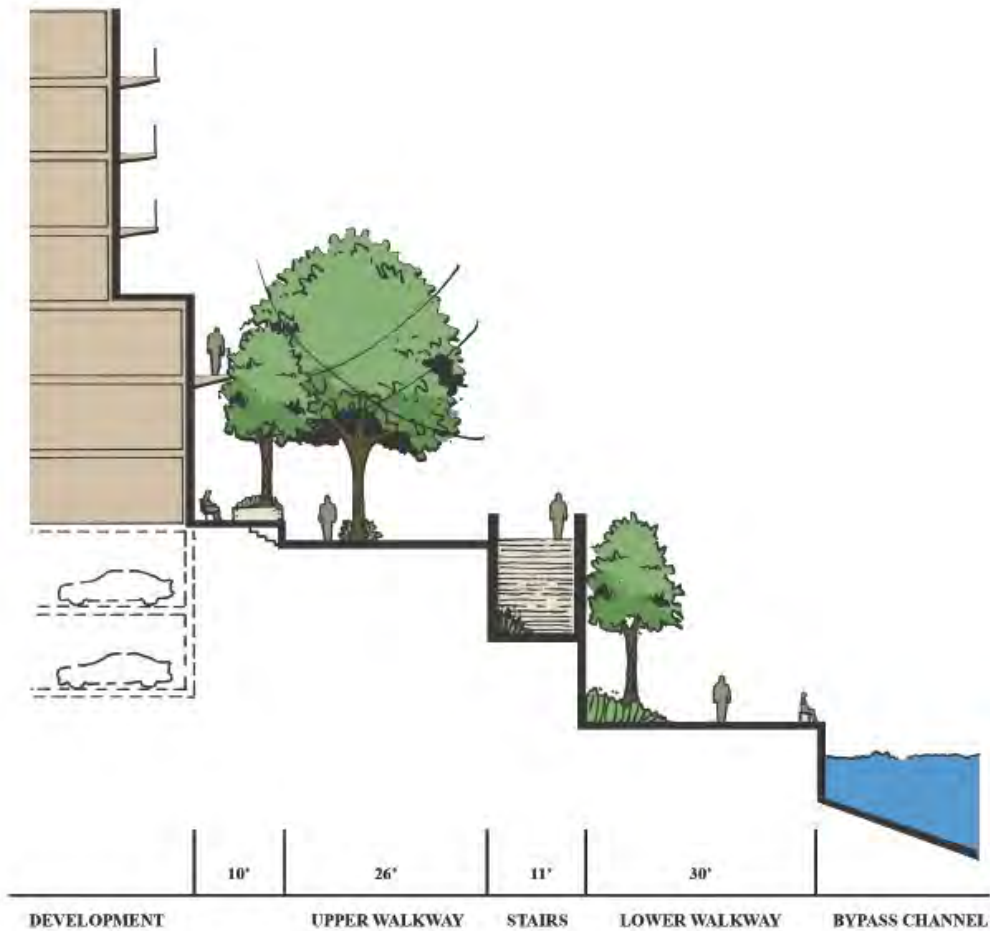
- Provide an 8-foot wide pedestrian area.
- Provide at least 3' of freeboard between the edge of the canal and the finished floor of development, outdoor dining or open space. (see page 23)
- Canals shall vary in width (from 19'- 32') throughout the canal zone depicted above.
- Provide continuous public access along canals, at least on one side.

### Guidelines:

- Encourage balconies and other features that provide visual interest and a strong relationship between buildings and canal walkways.



## BYPASS CHANNEL– TYPICAL ACCESS SECTION SOUTH OF CENTRAL CONTROL GATE

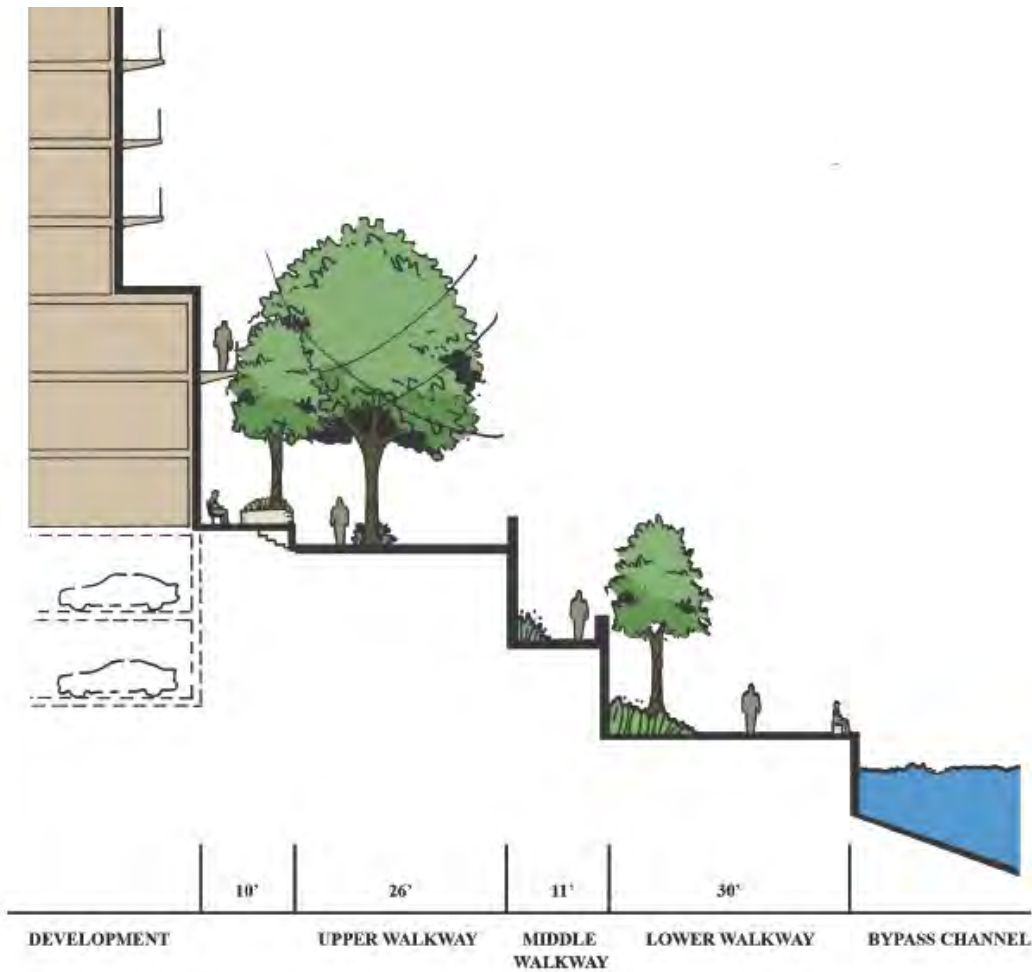


### Standards:

- Provide upper walkway according to USACE standards.
- Provide continuous public access along the upper and lower walkways.



## BYPASS CHANNEL– TYPICAL THREE WALKWAY SECTION SOUTH OF CENTRAL CONTROL GATE

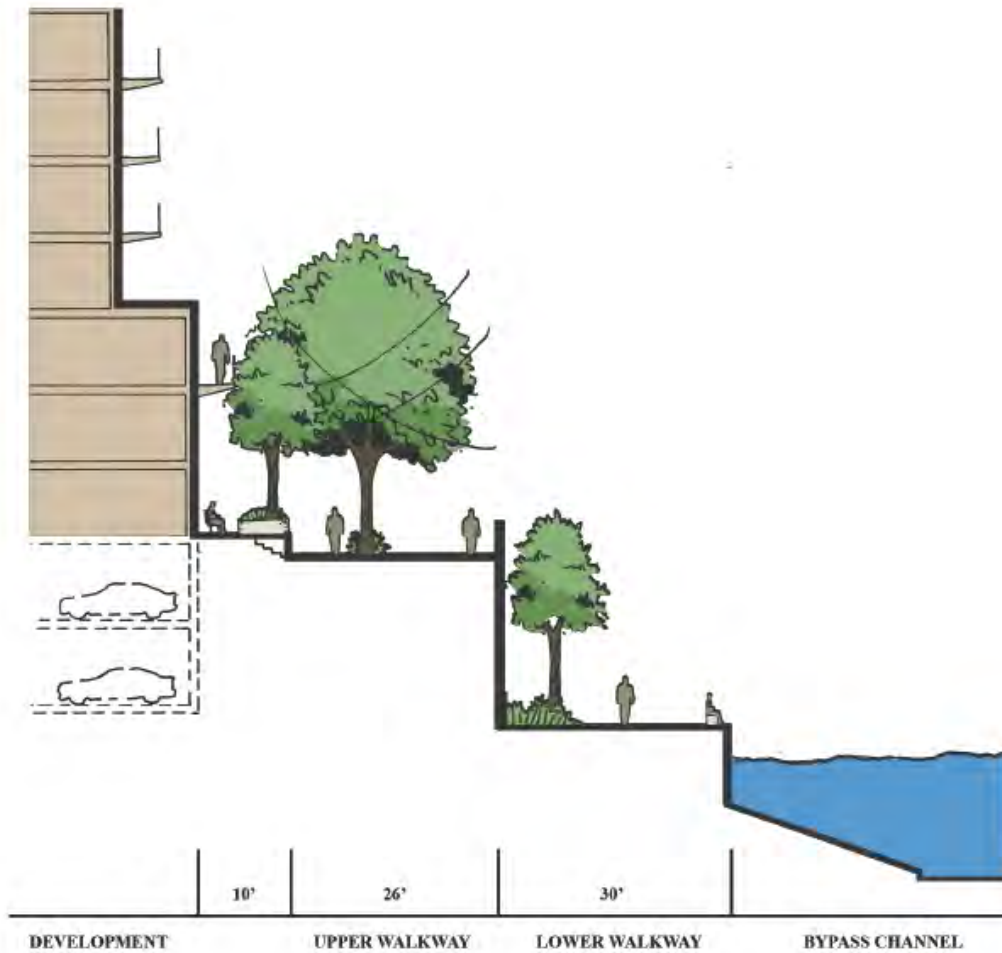


### Standards:

- Provide upper walkway according to USACE standards.
- Provide continuous public access along the upper and lower walkways.



## BYPASS CHANNEL– TYPICAL ACCESS SECTION NORTH OF CENTRAL CONTROL GATE



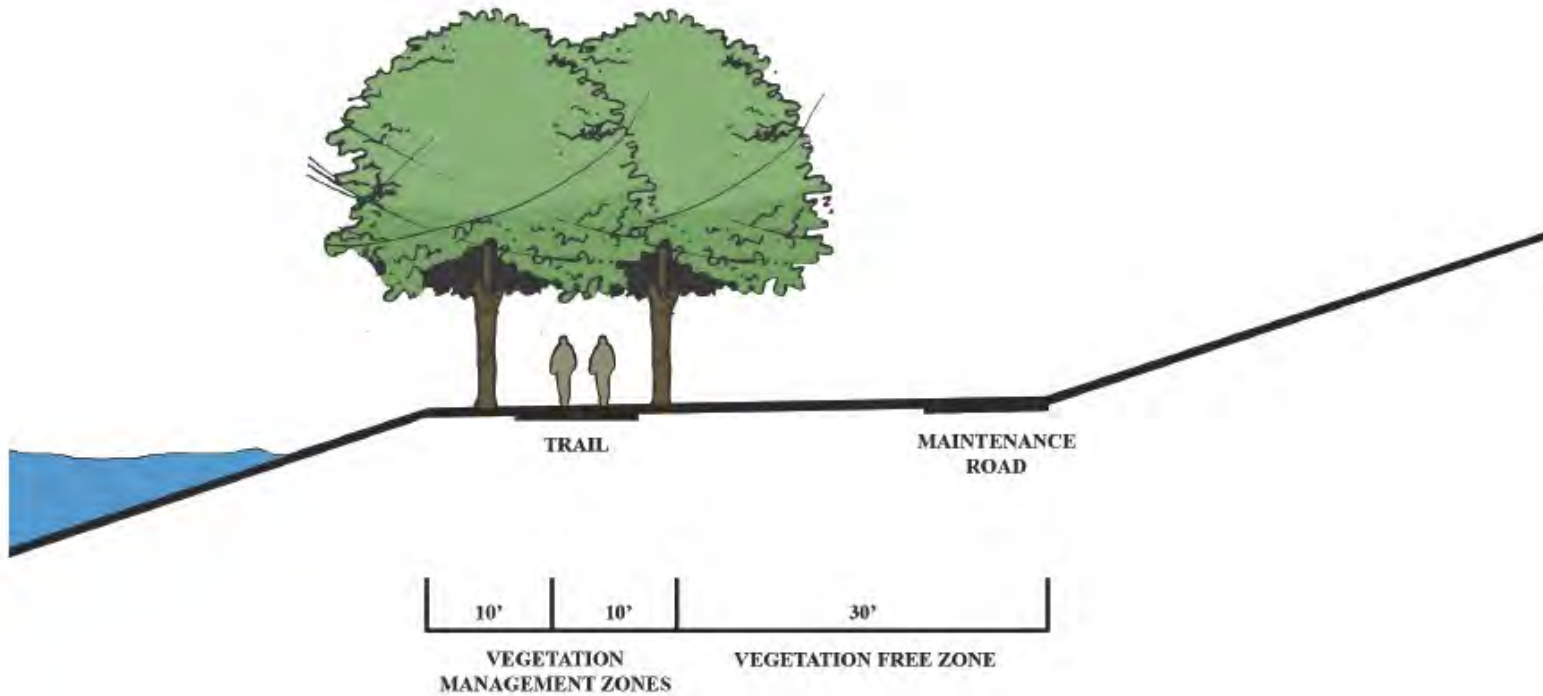
### Standards:

- Provide upper walkway according to USACE standards.
- Provide continuous public access along the upper and lower walkways.





## BYPASS CHANNEL- NORTHWEST BANK SECTION



**Note:**

The northwest bank of the Bypass Channel is often referred to as the “soft side”. This side is a earthen sloped levee and will restore a more natural greenbelt condition to the proposed urban area. The northwest side will have a trail that is connected to the existing Trinity Trail greenbelt. This will allow for continuous recreation on the soft side of the Bypass Channel.

